

ABERDEEN CITY COUNCIL

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COMMITTEE	<b>Enterprise Planning &amp; Infrastructure</b>
DATE	<b>7 September 2010</b>
DIRECTOR	<b>Gordon McIntosh</b>
TITLE OF REPORT	<b>Osborne Place Culvert Structural Improvements</b>
REPORT NUMBER:	<b>EPI/10/191</b>

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#### 1. PURPOSE OF REPORT

The purpose of this report is to inform Members of options to protect the structural integrity of a culvert located on Osborne Place and to gain Committee approval to proceed towards implementation of Option 3 highlighted within this report.

#### 2. RECOMMENDATION

It is recommended that the Committee approve Option 3 within this report and instruct officers to proceed towards the implementation of the scheme.

#### 3. FINANCIAL IMPLICATIONS

There are financial implications for all options identified within this report and are listed below.

Option 1 – Replacement of existing structure with reinforced concrete slab  
-approximately £160,000

Option 2 – Reinforced concrete slab strengthening – approximately £120,000

Option 3 – Installation of Build Outs and Bollards – approximately £18,000

There is sufficient funding within the 2010 / 2011 Weak Bridges Capital Budget to implement Option 3 improvements to Osborne Place culvert however it should be noted funding would be required prior to the implementation of Option 1 and Option 2.

#### 4. SERVICE & COMMUNITY IMPACT

Single Outcome Agreement 2008 – 2011 National Outcome 10 states ACC will continue to invest in proper maintenance of roads, pavements and street lights (VDFL)

The vision for Aberdeen City's Local Transport Strategy (LTS) is to develop "A sustainable transport system that is fit for the 21st Century, accessible to all, supports a vibrant economy and minimises the impact on our environment". Within the LTS it is highlighted that we wish to improve the condition of the

road, footway and cycle networks and to ensure a safe and secure transport system.

Local residents affected by the works were consulted informally and their views together with our response are included in Appendix A.

## 5. OTHER IMPLICATIONS

In 2000 a three tonne weight restriction was installed on the westernmost section of Osborne Place between Blenheim Lane and Blenheim Place. The implementation of Option 3 would include revoking the existing three tonne weight restriction.

## 6. REPORT

### **Background**

The section of Osborne Place culvert is approximately 24 metres in length and carries the Denburn watercourse under Osborne Place. The structure comprises a 3.7 metre single span culvert comprising a steel beam and concrete jack arch deck with concrete abutments and a concrete culvert.

A culvert assessment was carried out in 2000 and revealed corrosion of the steel beams which were unable to sustain heavier loading. Therefore temporary traffic restrictions and appropriate signage for a three tonne weight limit was introduced on Osborne Place between Blenheim Lane and Blenheim Place.

On 25 May 2004 a report was submitted to the, then named, Environment & Infrastructure Committee requesting officers to carry out preliminary statutory consultation on a proposal to install two build-outs covering the culvert section of Osborne Place. The proposal was refused as there were a number of objections from local residents in the area concerned over the reduction in car parking spaces which would be caused by the installation of build-outs.

On 10 March 2009 a confined space inspection was carried out in Osborne Place culvert. The inspection found a significant delamination, separation of layers, of approximately 50% of the main steel beams, particularly at the bearings, resulting in a loss of section.

Osborne Place is one of the streets within Controlled Parking Zone L and has a mix of residents only parking bays and pay and display bays within the street. In the section of Osborne Place between Prince Albert Street and Blenheim Place there are approximately 53 resident only parking bays (265 metres) and approximately 18 pay and display bays (90 metres).

### **Option Appraisal**

#### Option 1 - Replacement of complete structure

This option requires the complete removal of the existing concrete slab and steel beams and replaced with a reinforced concrete slab. This option would cause significant disruption for residents during the construction phase and would result in parking spaces temporarily being removed for approximately a two month period.

Estimated cost of the scheme is £160,000

#### Option 2 – Concrete slab strengthening

This option requires the removal of the fill and construction of a thin reinforced concrete slab on top of the existing members. This option would cause significant disruption for residents during the construction phase and would result in parking spaces temporarily being removed for approximately a two month period.

Estimated cost of the scheme is £120,000.

#### Option 3 – Installation of Build –Outs (See attached Appendix B)

This option will result in the installation of one build-out located on the southern side of Osborne Place, outside numbers 109– 111, and one build-out on the northern side of Osborne Place outside numbers 152 / 154. The installation of the build-outs would result in the removal of three residents' only parking spaces between 109 – 111 Osborne Place and removal of two residents' only parking spaces outside 152 Osborne Place.

It is proposed to alleviate the removal of the residents' only parking spaces by reallocating the 5 spaces within the existing pay and display sections located between 113 Osborne Place and Blenheim Place and 152 Osborne Place and Blenheim Place. This would result in the same number of residents' only parking spaces but the reduction of 5 pay and display spaces.

Estimated cost of the scheme is £18,000

#### 7. REPORT AUTHOR DETAILS

Scott Ramsay, Technical Officer

Email: [sramsay@aberdeencity.gov.uk](mailto:sramsay@aberdeencity.gov.uk)

Telephone: 01224 523463

#### 8. BACKGROUND PAPERS

[http://councilcommittees/acc\\_data/committee%20reports/cs\\_env\\_r6\\_2\\_040525.pdf](http://councilcommittees/acc_data/committee%20reports/cs_env_r6_2_040525.pdf)

## APPENDIX A

### Residents Comments

#### **No. 146 Osborne Place - Mrs Murray**

**She was concerned regarding the underpinning of the Osborne Place properties and the bin collections.**

She was assured this would not be affected by the works.

She was also informed that on completion of the works, the 3T weight restriction and redundant signs would be removed.

#### **No. 113 Osborne Place – Paul Arnell.**

**After being given complete details of all options, he stated that he preferred Option 1 as “that appears to be the only option that fully and properly addresses the underlying problem”**

**In addition he wished to know the proper procedure for making his feelings known to the Committee – he was duly informed.**

#### **Residents ‘X’**

**The residents requested a drawing of the culvert together with an assessment of whether the loss of section would have a structural impact on their property.**

A copy of the Culvert Inspection Report and a location plan was provided – as the culvert does not pass under No.150 Osborne Place there would be no structural impact.

**They also sought clarification as to why the repair of the culvert is not the preferred option and why the works are not urgent.**

They were informed that the build-out option was chosen on the basis of value for money and the works are not urgent as a 3T weight restriction is currently in place.

**They asked if there would be a formal consultation and if there would be an appeals procedure.**

They were informed that there would be no formal consultation and the decision of the Committee would be final, however they could request permission to address the Committee to express their views.

#### **No. 111 Osborne Place – Mr & Mrs Cartney.**

**They queried why the repair of the culvert was not the preferred option.**

They were informed that the decision to install build-outs was the most cost effective option.

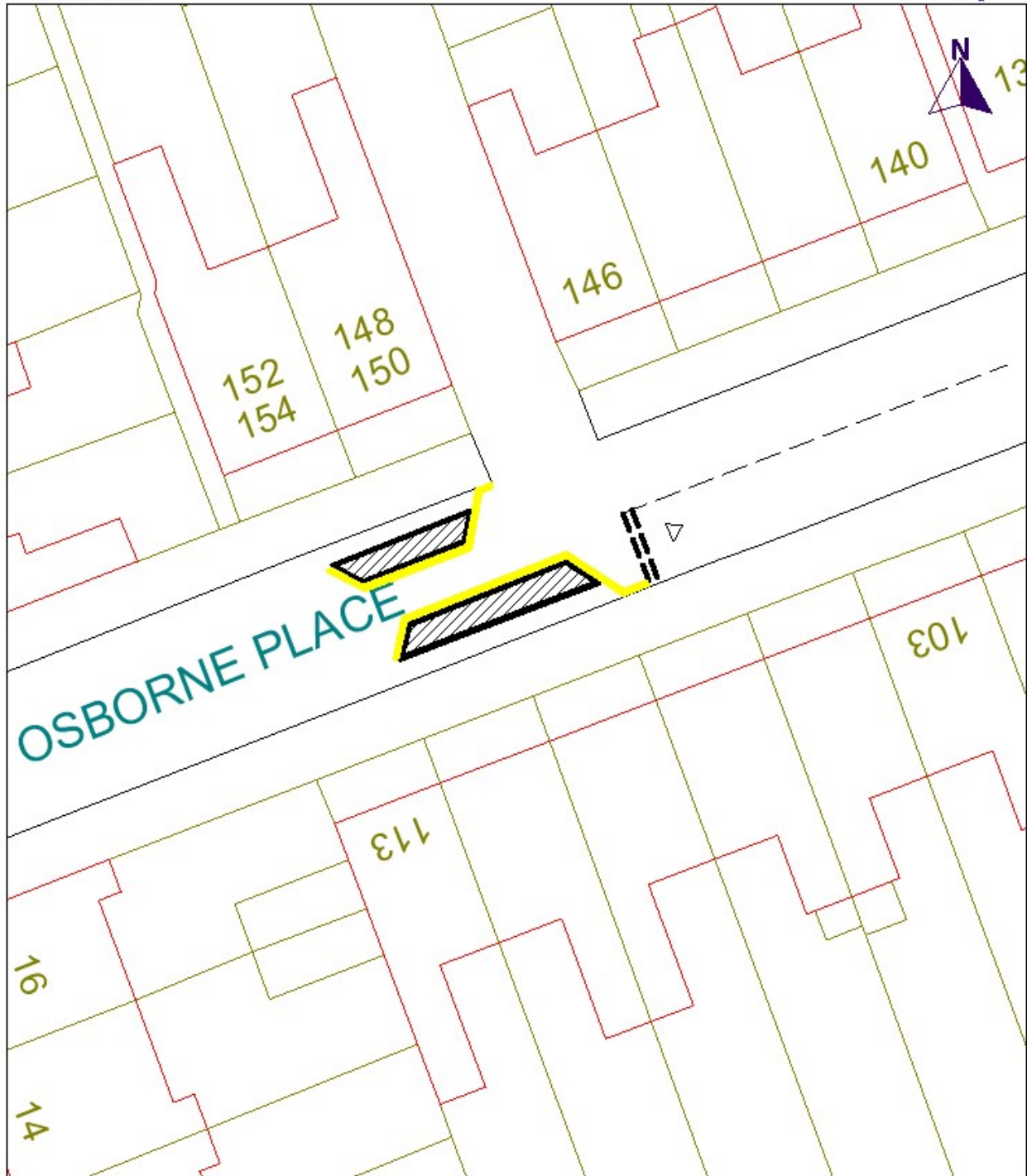
**They also stated that they were concerned at the relocation of parking spaces away from their front door – and requested that their views on the above be put to the Committee.**

They were informed that the relocation of parking spaces was regrettable but unfortunately unavoidable.

**APPENDIX B**



**GEOGRAPHICAL  
INFORMATION  
SYSTEM**



**Title:** OSBORNE PLACE CULVERT IMPROVEMENTS  
PROPOSAL FOR INSTALLATION OF BUILD OUTS

**Scale:** 1:300

**Date:** 23 June 2010

**Map Ref:** NJ9205NE



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